

CARTERET COUNTY
DB00545

PROJECT REFERENCE NO.	SHEET NO.
DB00545	1

WBS# 2023CPT.02.01.10161



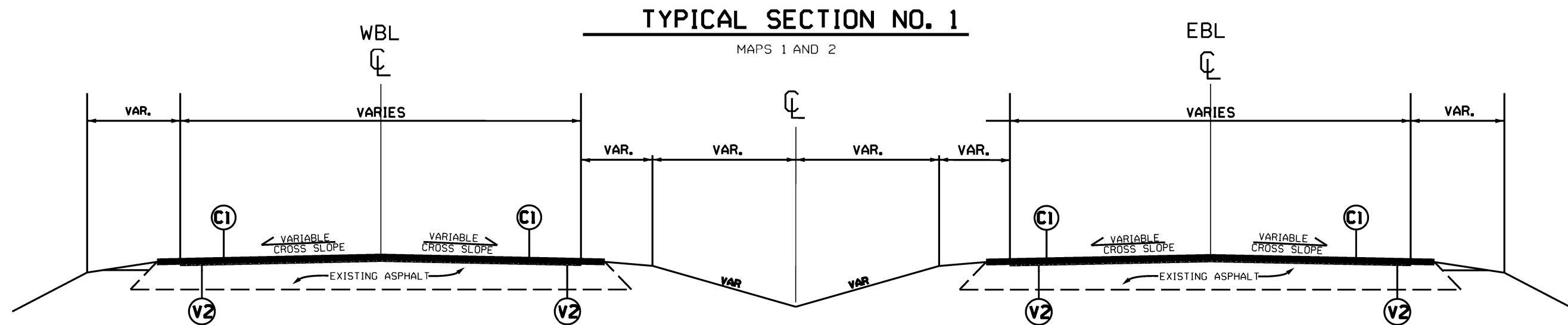
NCDOT
DIVISION 2

TYPE OF WORK: MILLING AND RESURFACING



PAVEMENT SCHEDULE	
C1	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
C2	PROP. APPROX. 3" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C AT AN AVERAGE RATE OF 168.0 LBS. PER SQ. YD. PER LIFT. THIS SURFACE COURSE IS TO BE APPLIED IN TWO LIFTS 1 1/2" EACH FOR A TOTAL OF 3"
T	SHOULDER RECONSTRUCTION
V1	INCIDENTAL MILLING.
V2	MILLING DEPTH 2" FOR THE ENTIRE WIDTH OF ROADWAY.
V3	MILLING DEPTH 3" FOR ENTIRE WIDTH OF ROADWAY.
DRAWINGS NOT TO SCALE	

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

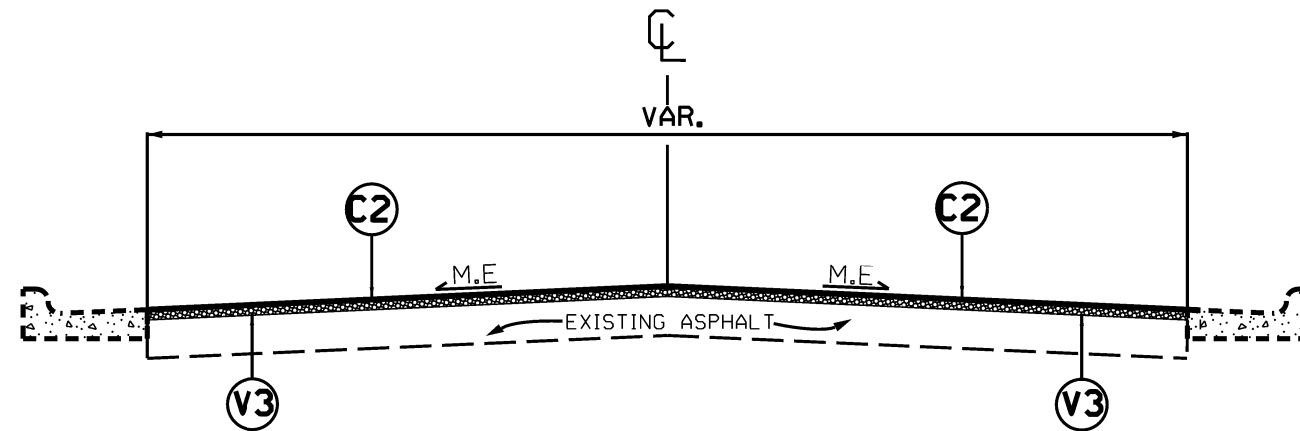


NOTE:

1. MILL FULL WIDTH OF THE ENTIRE ROADWAY TO A DEPTH OF 2 INCHES, MILLING TO INCLUDE BOTH NCDOT AND CITY SIDE STREETS TO THE BACK OF THE RADIUS.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF THE EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
3. INCLUDES INCIDENTAL MILLING AT THE ENDS OF THE MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.

TYPICAL SECTION NO. 2

MAP 3

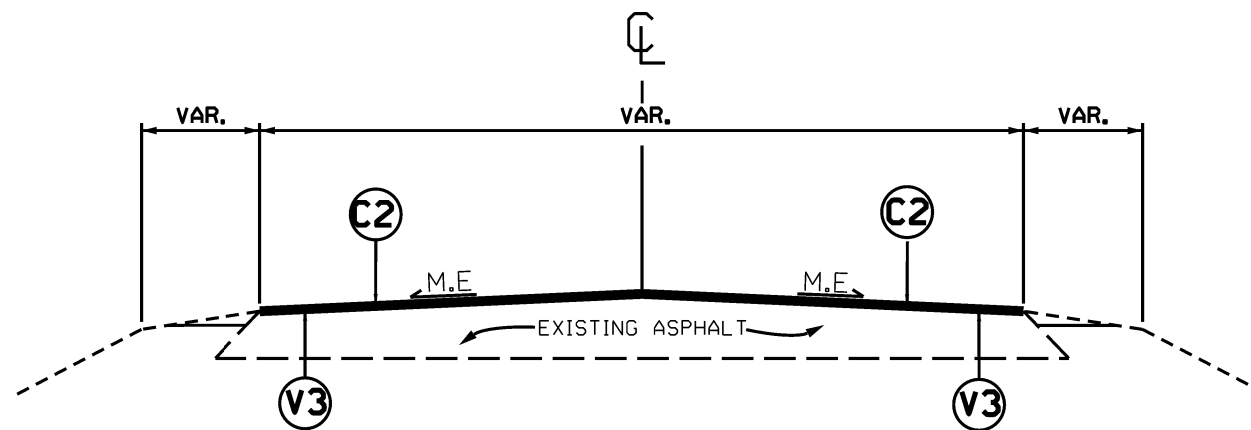


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TYPICAL SECTION NO. 3

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PROJECT NO.	SHEET NO.	TOTAL NO.
DB00545	4	

SUMMARY OF QUANTITIES

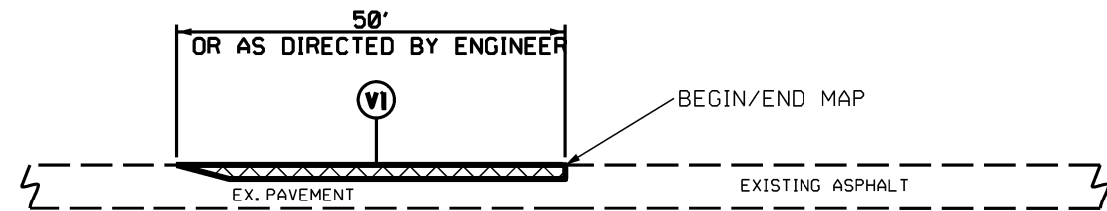
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	1220000000-E	1297000000-E	1330000000-E	1523000000-E	1575000000-E	2613000000-N	2752000000-E		4413000000-N	4457000000-N	4510000000-N		
												INCIDENTAL STONE BASE	2" MILLING	3" MILLING	INCIDENTAL MILLING	SURFACE COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	CONCRETE CURB RAMP - REMOVE/REPLACE	2'6" CURB & GUTTER - REMOVE/REPLACE	CONCRETE VALLEY GUTTER - REMOVE/REPLACE	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT	
											MI	FT	TONS	SY	SY	SY	TONS	TONS	EA	LF	LF	SF	LS	HR
2023CPT.02.01.10161	Carteret	1	US 70 WESTBOUND	FROM 5 LANE TO 550' W OF SR 1147	1	2	MU	NO	NO	2	42	100	49,280		3,900	4,500	270			230	0.350	40		
TOTAL FOR MAP NO. 1											2		100	49,280		3,900	4,500	270			230	0.350	40	
2023CPT.02.01.10161	Carteret	2	US 70 EAST BOUND	FROM 550' W OF SR 1147 TO 5 LANES	1	2	MU	NO	NO	2.01	51	100	60,000		2,000	5,500	330		19		230	0.320	40	
TOTAL FOR MAP NO. 2											2.01		100	60,000		2,000	5,500	330		19		230	0.320	40
2023CPT.02.01.10161	Carteret	3	US 70 (5 LANE SECTION)	FROM 5 LANE TO SR1605	2,3	2	MU	NO	NO	2.16	80	108		100,000	4,250	18,000	1,080	7	532	15	250	0.330	40	
TOTAL FOR MAP NO. 3											2.16		108		100,000	4,250	18,000	1,080	7	532	15	250	0.330	40
TOTAL FOR PROJ NO. 2023CPT.02.01.10161											6.17		308	109,280	100,000	10,150	28,000	1,680	7	551	15	710	1.000	120
GRAND TOTAL														209,280						566				

CURB & GUTTER	STA.	STA.	LENGTH	MAP
	0+30	0+40	19' LT	2
	47+15	47+40	25' RT	3
	49+50	49+73	23' RT	3
	52+00	52+02	2' LT	3
	51+87	52+27	40' RT	3
	53+73	54+03	30' RT	3
	55+13	55+41	28' RT	3
	56+79	57+30	51' RT	3
	56+85	57+30	45' LT	3
	62+86	63+19	33' LT	3
	65+96	66+55	59' LT	3
	67+01	67+22	21' RT	3
	69+49	69+63	14' LT	3
	75+94	76+36	42' LT	3
	78+84	78+98	14' RT	3
	84+49	84+91	42' LT	3
	108+05	108+36	31' LT	3
	110+14	110+32	18' LT	3
	112+52	112+66	14' RT	3

CURB RAMPS	QUANTITY	MAP
Raliegth Ave.	2	3
Savanah Ave.	1	3
Hickory Dr.	2	3
Cedar St.	2	3

VALLEY GUTTER	STA.	STA.	LENGTH	MAP
	51+87	52+02	15' LT	3

MILLING TYPICALS



DETAIL 1
BEGIN/END MAP TIE-IN

NOTE:

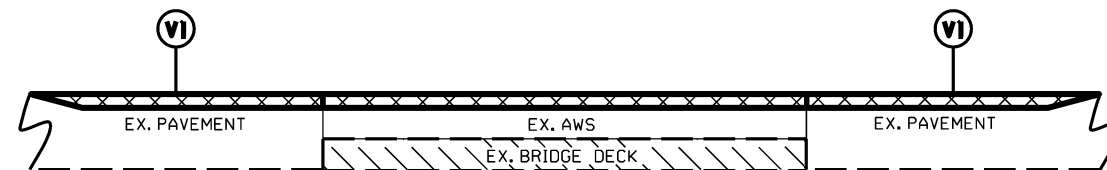
- MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



DETAIL 2
BRIDGE MILLING

NOTE:

- MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.

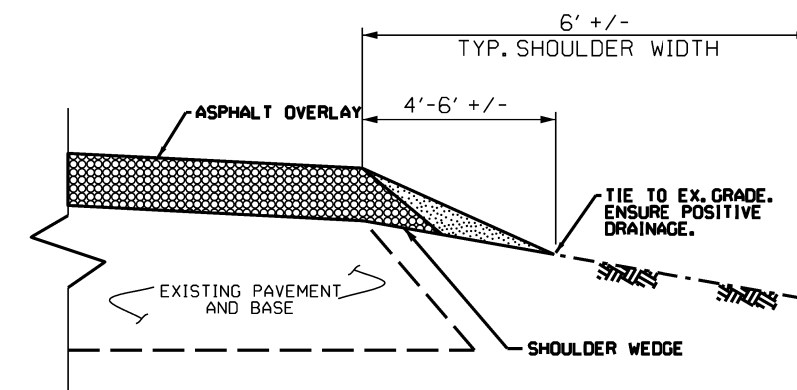


DETAIL 3
BRIDGE MILLING

NOTE:

- INCLUDES MILLING FOR THE ENTIRE WIDTH OF THE BRIDGE WEARING SURFACE, AS DIRECTED BY THE ENGINEER.

SHOULDER RECONSTRUCTION TYPICAL

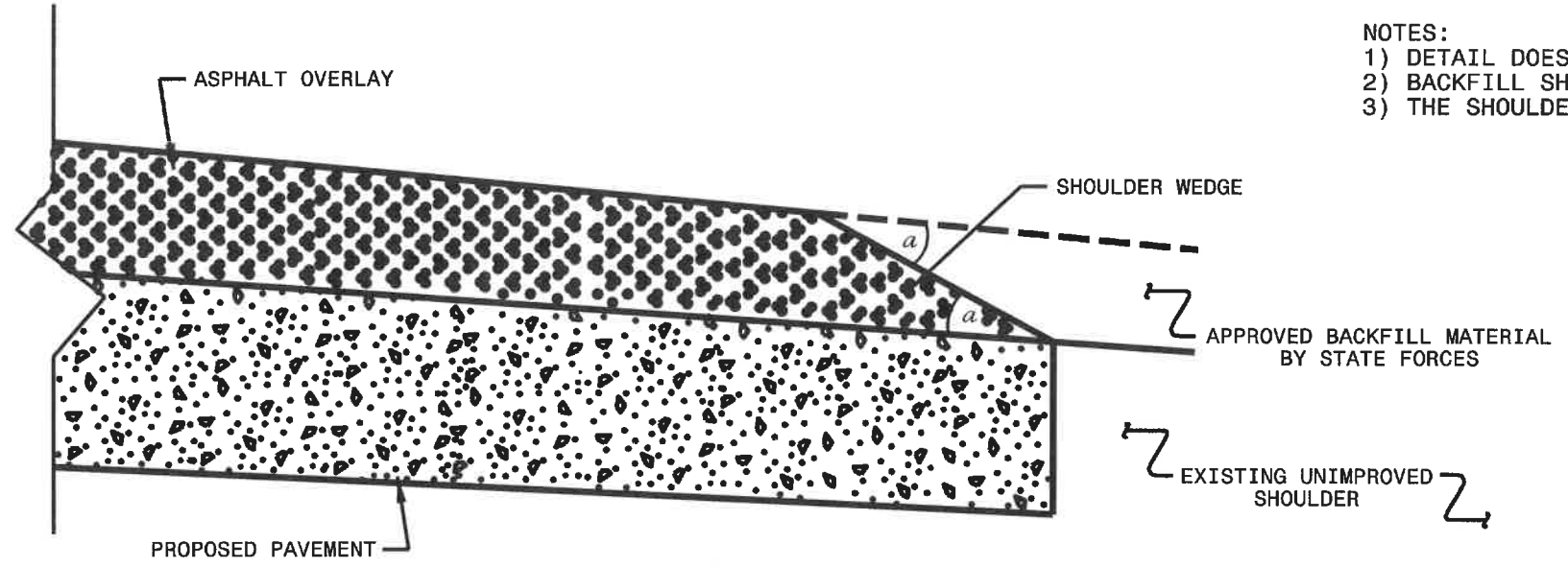


SHOULDER RECONSTRUCTION DETAIL

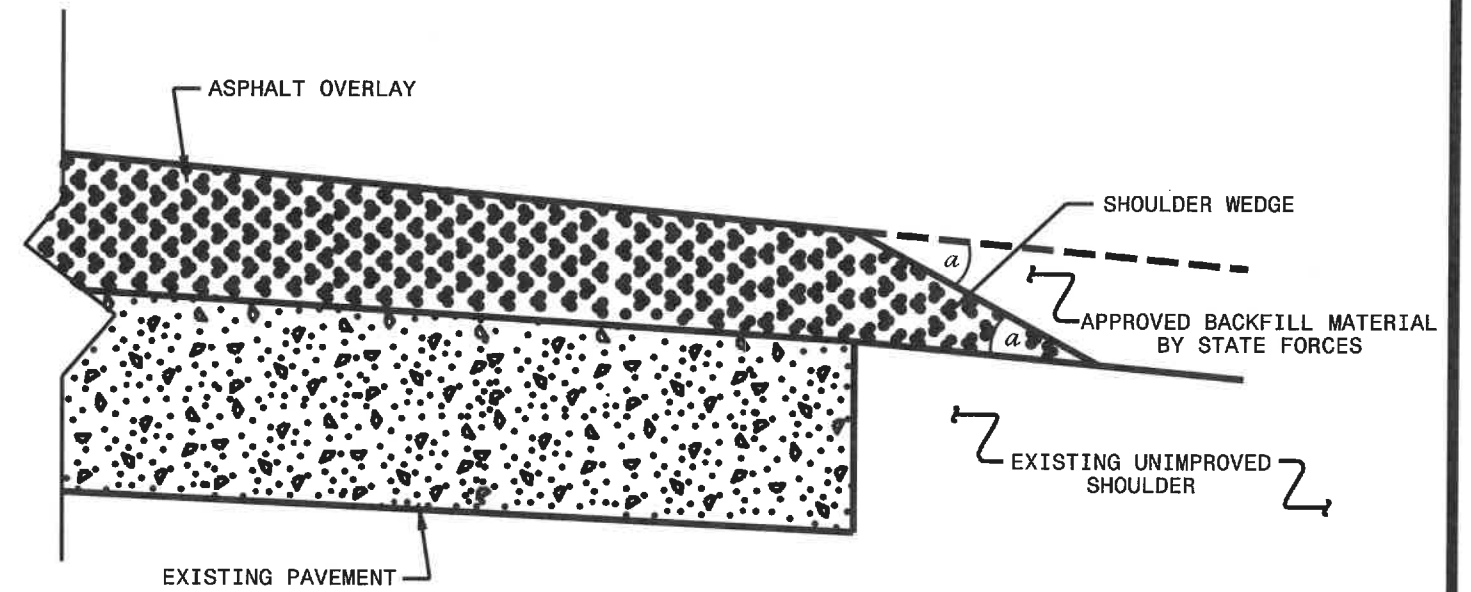
NOTE:

- SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
- A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
- REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

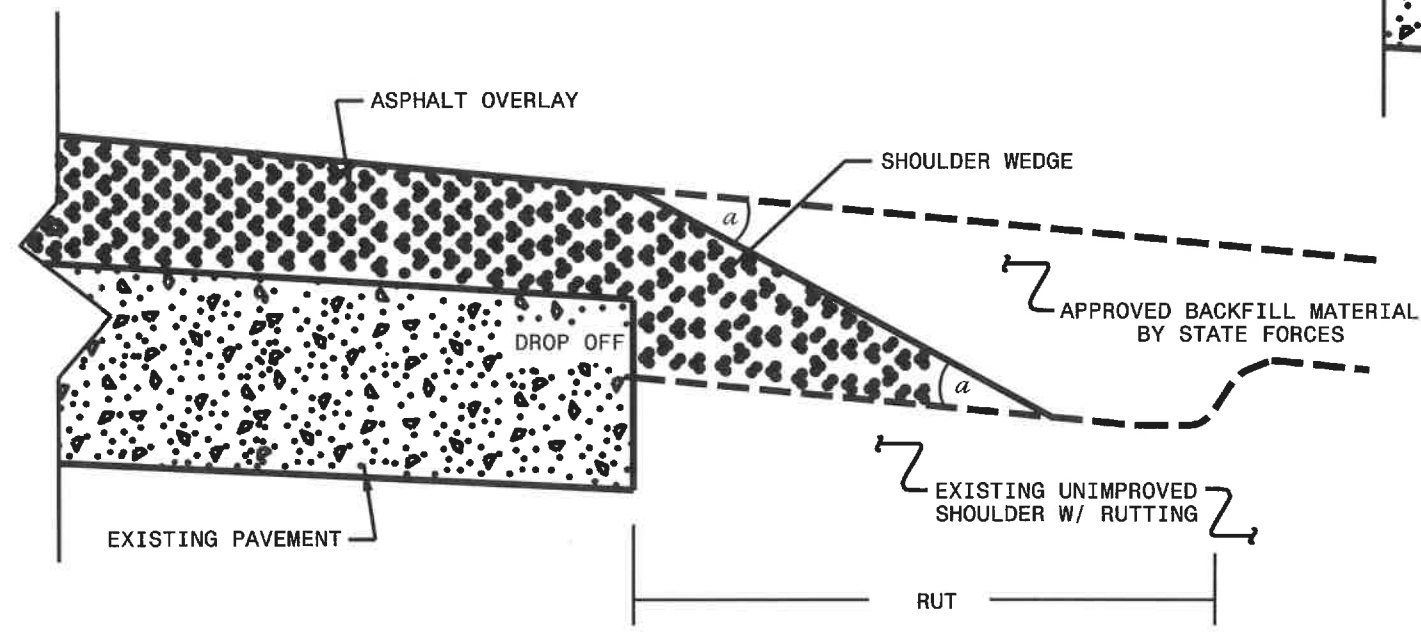
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFB AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

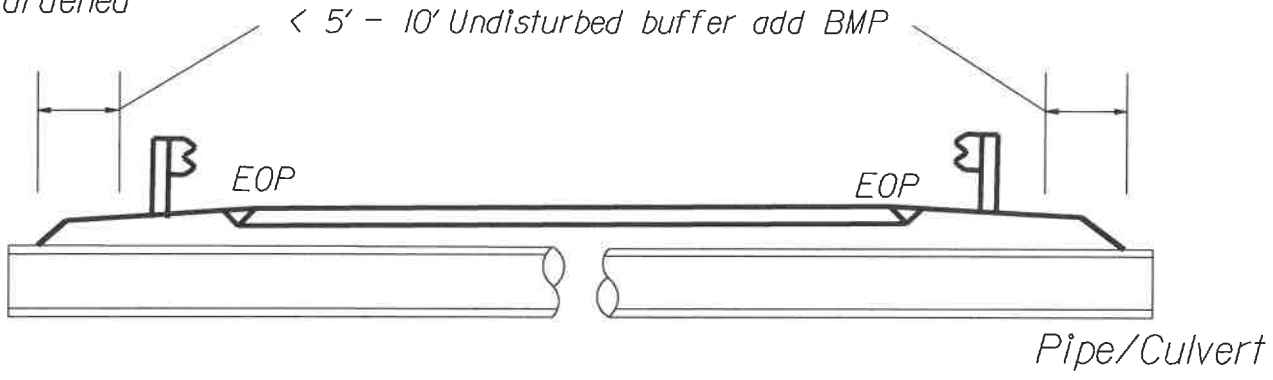
- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T.SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC: s:\usr\details\stand\shoulder_wedge\std\std.dgn	

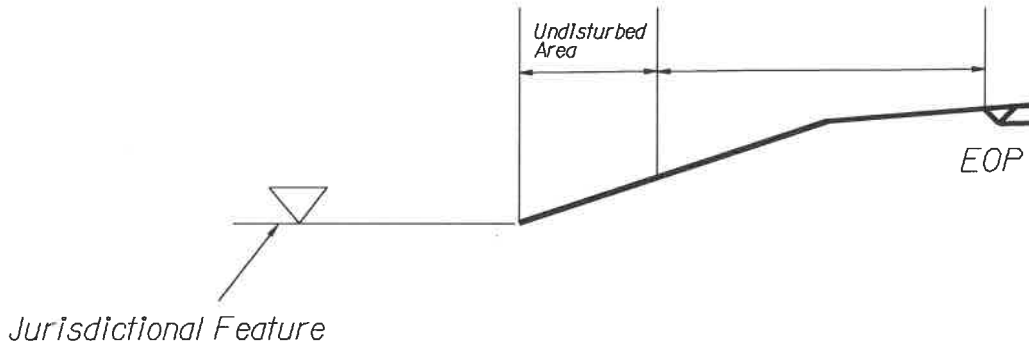
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle, Silt Fence or Hardened Aggregate.

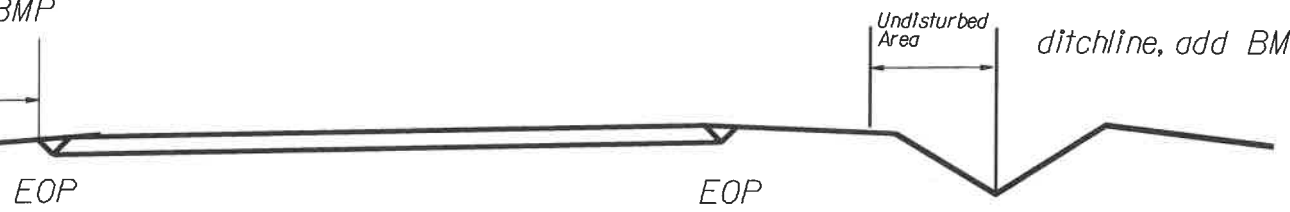
EROSION CONTROL DETAIL



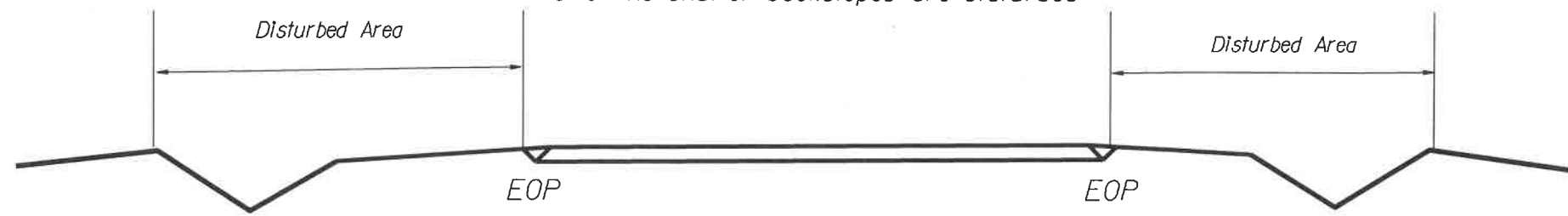
< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP



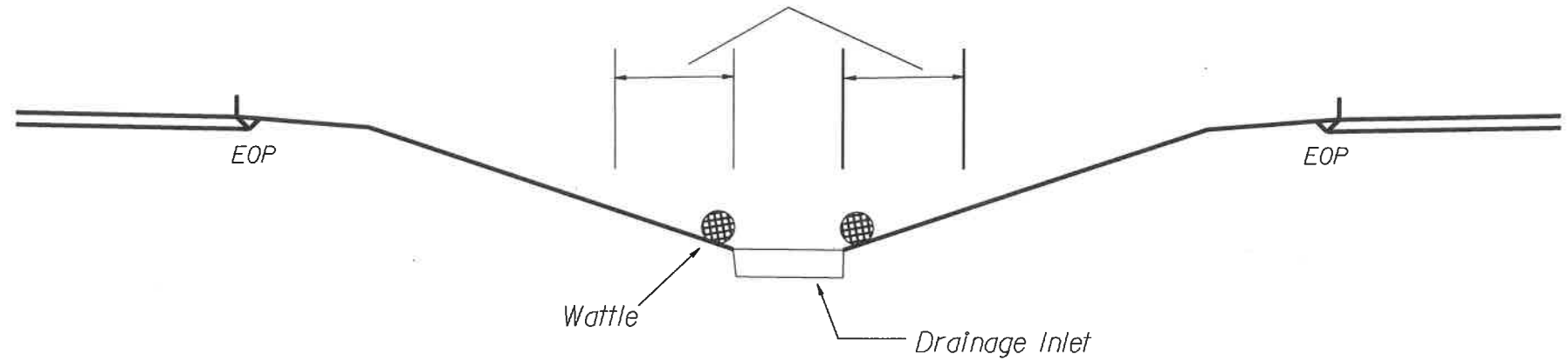
< 5' - 10' Undisturbed buffer from ditchline, add BMP



Use BMP's if shoulders and/or front slopes and/or ditchline and/or backslopes are disturbed

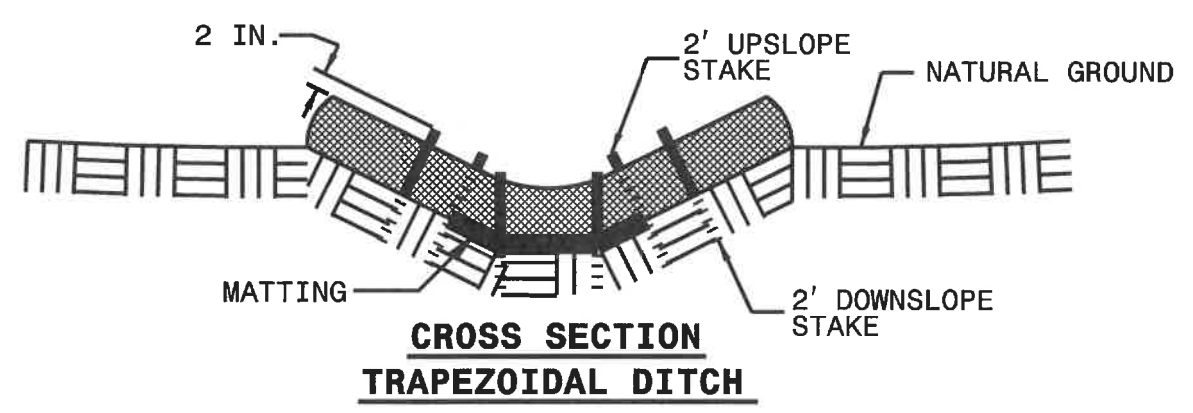
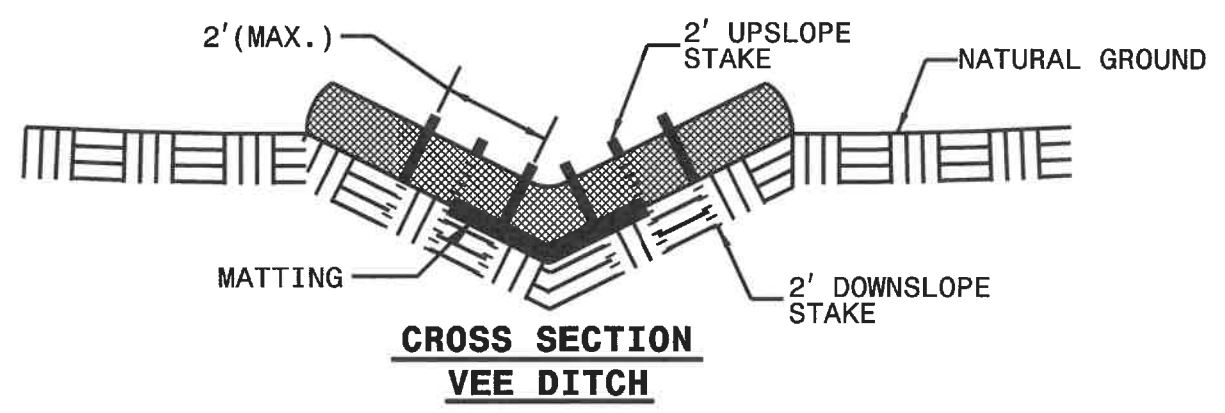
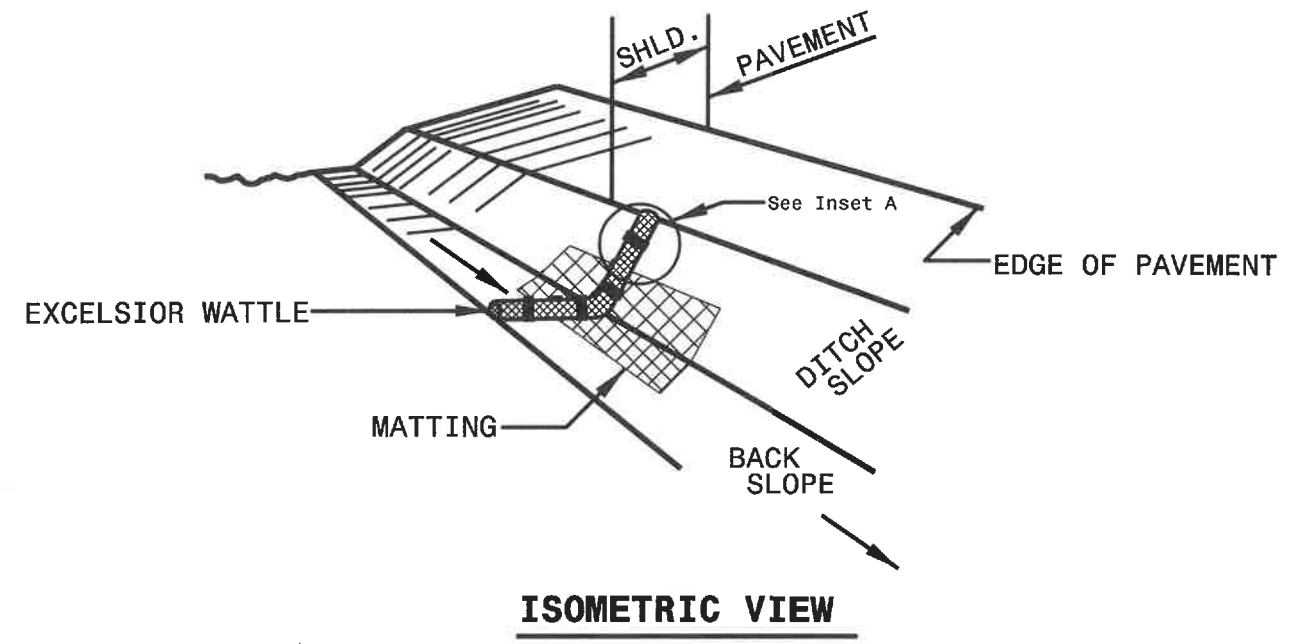


< 5' - 10' Undisturbed buffer from inlet, add wattle



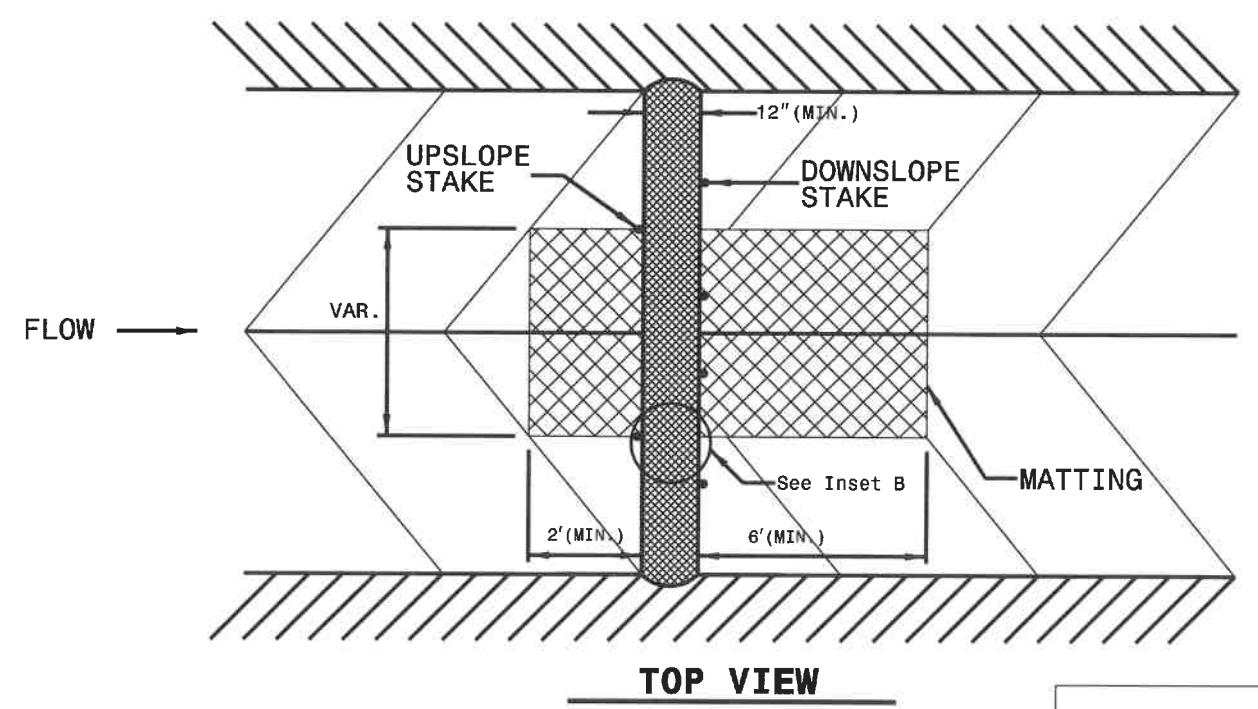
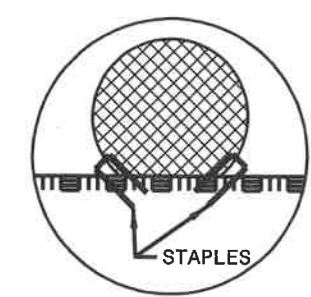
NOT TO SCALE

WATTLE DETAIL



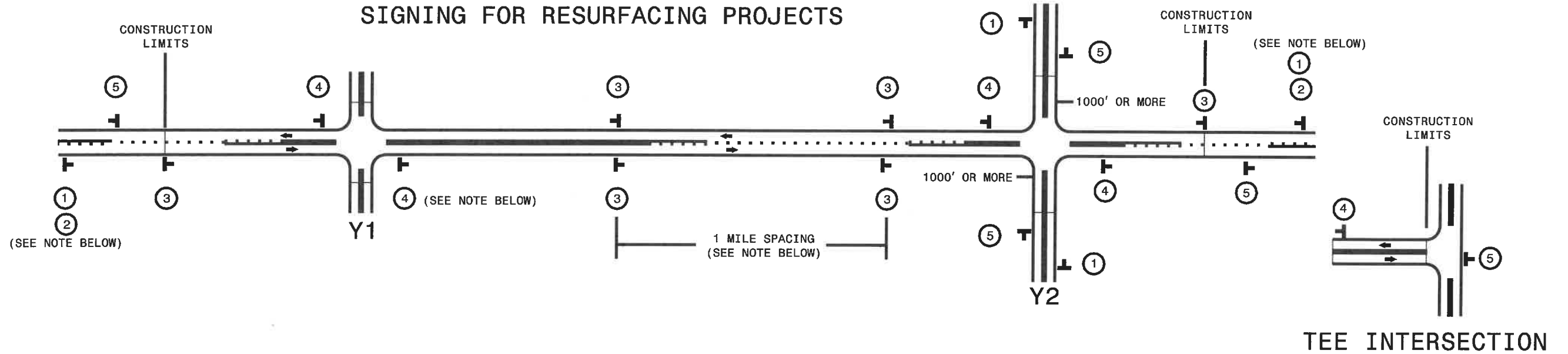
NOTES:

- USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.
- USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
- ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
- INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
- PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.
- INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
- INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



NOT TO SCALE

SIGNING FOR RESURFACING PROJECTS



MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	 	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;"> <small>W20-7 A 48" X 48"</small> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
		<p>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.</p> <p>- AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</p>	
		<p>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS.</p> <p>- DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</p> <p>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</p> <p>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</p> <p>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p> <p>- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</p>	
		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</p>	



RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS